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Canada Marine Dept., qd

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(ISSUED BY THE DEPARTMENT OF MARINE)

LOAD LINE REGULATIONS

(Approved by Order in Council dated November 7, 1932)



OTTAWA
P. A. ACLAND
PRINTER TO THE KING'S MOST EXCELLENT MAJESTY
1932



ISSUED BY THE DEPARTMENT OF MARINE

LOAD LINE REGULATIONS

(Approved by Order in Council dated November 7, 1932)

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OTTAWA
F. A. ACLAND
PRINTER TO THE KING'S MOST EXCELLENT MAJESTY
1932

P.C. 2448

AT THE GOVERNMENT HOUSE AT OTTAWA

MONDAY, the 7th day of November, 1932.

PRESENT:

HIS EXCELLENCY THE GOVERNOR GENERAL IN COUNCIL

WHEREAS an International Convention respecting Load Lines was signed on behalf of Canada at London on the 5th day of July, 1930;

AND WHEREAS the provisions of the Convention have been confirmed and sanctioned by the Safety of Life at Sea and Load Line Conventions Act, 1931, being Chapter 49 of the Statutes of 1931;

AND WHEREAS the Acting Minister of Marine, on the advice of the Assistant Deputy Minister of Marine, recommends the establishment of the annexed regulations, which are designed to give effect to the requirements of the Load Line Convention;

THEREFORE His Excellency the Governor General in Council, under and in virtue of the provisions of Section 4 of the said Act, is pleased to make the annexed load line regulations, and they are hereby made and established accordingly.

E. J. LEMAIRE,
Clerk of the Privy Council.

LOAD LINE REGULATIONS

1. In these regulations, unless the context otherwise requires,
 - (a) The "Act" means the Safety of Life at Sea and Load Line Conventions Act, 1931.
 - (b) "Convention" means the International Convention respecting Load Lines.
 - (c) "Minister" means the Minister of Marine.
 - (d) "Assigning Authority" means the Minister of Marine, Lloyd's Register of Shipping, the British Corporation Register of Shipping and Aircraft, or the British Committee of the Bureau Veritas.
 - (e) "Surveyor" means a person appointed under the provisions of Section 550 of the Canada Shipping Act to inspect hulls and equipment, or, where the Assigning Authority is one of the classification societies mentioned in paragraph (d), a surveyor employed by such society.
 - (f) "Chairman" means the Chairman of the Board of Steamboat Inspection, appointed under the provisions of Section 560 of the Canada Shipping Act.

2. The Minister shall be charged with the administration of the Act, and of these regulations.

3. Lloyd's Register of Shipping, the British Corporation Register of Shipping and Aircraft, and the British Committee of the Bureau Veritas are hereby authorized to survey and mark ships under the provisions of these regulations, and to issue load line certificates.

4. (1) Every application for the issue or renewal of a load line certificate shall be made by, or on behalf of the owner of a ship, to an Assigning Authority.

(2) On every such application there shall be paid by the owner the fee prescribed in the first schedule to these regulations.

5. (1) The Assigning Authority shall, upon receipt of the application and the prescribed fee, cause the ship to be surveyed by a surveyor as hereinafter provided.

(2) The surveyor shall survey the ship for the purpose of obtaining the information required for the assignment of load lines under the provisions of the Convention, and with a view to satisfying himself:—

- (a) that the material and workmanship of all parts of the hull of the ship are in all respects satisfactory and efficient, and that the hull is in good condition internally and externally
- (b) that the conditions of assignment of load lines imposed by the Convention have been complied with.

(3) For the survey required under this section the ship shall be placed in a dry dock, or on a slipway, and shall be opened out to the extent necessary to allow the surveyor to obtain all particulars and to satisfy himself as to the condition of the ship, provided, however, that if the ship is one coming under regular survey or inspection by the surveyors of the Assigning Authority, the last survey or inspection in dry dock may be accepted as the survey required under this section if, after examination afloat, the surveyor is satisfied as to the condition of the ship. In such case the period for which the certificate may be issued shall date from the last survey or inspection in dry dock.

(4) It shall be the duty of the owner of a ship to provide all facilities for the survey required by this section.

(5) At every survey in dry dock or on a slipway for the assignment or renewal of load lines the surveyor shall verify the draught marks on the stem and sternpost of the ship.

(6) On the completion of the survey the surveyor shall forward to the Assigning Authority a report stating the result of the survey, and containing such particulars of the ship as are required by the Assigning Authority to enable it to assign the appropriate freeboards.

(7) On receipt of the surveyor's report the Assigning Authority, if satisfied that the ship complies with the appropriate provisions of the Convention, shall assign freeboards, and shall furnish the owners with particulars as to the nature of the load lines and of the position in which the deck line and the load lines are to be marked on the ship.

(8) On receiving from the Assigning Authority the particulars as to the deck line and load lines, as provided in subsection (7) of this section, the owner shall cause to be marked on each side of the ship the appropriate marks, to the satisfaction of the surveyor, in accordance with the Convention.

6. Where the Assigning Authority is the Minister, this shall be indicated by the marking of the letters "C.M." on the ship's sides, in accordance with the provisions of the Convention.

7. On the Assigning Authority being satisfied that the ship has been marked to the surveyor's satisfaction, as required under the Convention, the certificate shall be delivered to the applicant, together with a certified copy thereof. Except where the Assigning Authority is the Minister, a further certified copy shall be sent to the Minister.

8. Every load line certificate in respect of freeboards assigned to a ship shall be issued by the Assigning Authority by whom the freeboards were assigned. Where the Assigning Authority is the Minister, the certificate shall be registered by the Chairman, and shall be marked to show that it has been so registered.

9. (1) A load line certificate issued under the provisions of the Convention shall be in such one of the forms set out in the second schedule to these regulations as is appropriate in the case, and, if it be issued by an Assigning Authority other than the Minister, it shall be signed by such competent officer of that Authority as may be approved by the Minister.

(2) Where, in the opinion of an Assigning Authority, the delivery of a certificate to a ship in respect of which freeboards have been assigned would cause undue delay in sailing, such Assigning Authority may authorize the issue of a provisional load line certificate, which may be given over the signature of a surveyor employed by that Assigning Authority, but such certificate shall not be given unless the Assigning Authority is satisfied that the ship has been marked in accordance with the provisions of the Convention, and shall not be valid for more than three months from the date of issue.

(3) Any load line certificate which may be issued to a ship under the provisions of subsection (2), Section 13 of the Act, shall be in a form approved by the Minister.

10. Where a ship has been marked with the deck and load lines required under the Convention, and has been given a load line certificate, the owner or master shall keep the ship so marked during the period for which such certificate is given, and no person shall conceal, remove, alter, deface or obliterate, or suffer any person under his control to conceal, remove, alter, deface or obliterate any mark placed on the ship in accordance with the provisions of the Convention, except with the authority of a person entitled under the Act to authorize the alteration of the mark.

11. (1) Subject to the provisions of this section, the owner of every ship in respect of which a load line certificate has been issued, shall, during the period for which the certificate is issued, cause the ship to be surveyed at least once in each year after the issue of the certificate, for the purpose of seeing whether it should remain in force, having regard to the provisions of paragraph (3), Article 14 of the Convention.

(2) An Assigning Authority by whom any load line certificate has been issued, may extend such certificate for a period of two months without having the ship subjected to the annual survey required under this section, provided such Assigning Authority is satisfied:—

- (a) that insistence on the survey would interfere unreasonably with the business of the ship.
- (b) that the ship owner has not been lacking in due diligence to have the ship surveyed, and
- (c) that there is no reason to suppose that the conditions on which the load line certificate was granted have not been maintained.

In any particular case the Minister may grant a further extension for good cause.

(3) A load line certificate shall cease to be in force unless it is endorsed to show that the provisions of this section in respect of annual survey have been complied with, or unless there be produced authority, in writing, extending the survey period.

(4) Any ship attempting to proceed to sea with a certificate not endorsed in accordance with the provisions of this section, or in respect of which authority extending the survey period is not produced, shall be deemed to be attempting to proceed to sea without a certificate, and shall be detained accordingly.

12. (1) Every application for the survey of a ship under the provisions of the preceding section shall be made by, or on behalf of the owner, to the Assigning Authority by whom the certificate was issued.

(2) There shall be paid in respect of such survey the fee prescribed in the first schedule to these regulations.

(3) The Assigning Authority shall, upon receipt of the application and the prescribed fee, cause the ship to be surveyed by a surveyor who shall survey the ship with a view to satisfying himself:—

- (a) that the fittings and appliances for the protection of openings, the guard rails, the freeing ports, and the means of access to the crew's quarters, have been maintained in as effective a condition as they were when the certificate was issued.
- (b) that no material alterations have taken place in the hull, or the super-structures of the ship, which affect the position of the load lines.

(4) Upon the completion of the survey, to the satisfaction of the surveyor, he shall forward a report thereon to the Assigning Authority and endorse on the certificate a statement that the survey has been so completed.

13. Where a ship has had load lines assigned by an Assigning Authority other than the Minister, such Assigning Authority shall report to the Minister every case of neglect on the part of the owner to have such ship submitted for annual survey under the provisions of Section 11, after the period of grace provided for in that section has expired.

14. A load line certificate shall cease to have force if, due to damage to the hull or superstructures, or to any of the appliances or fittings required under the provisions of the Convention, the conditions under which such certificate was issued have materially altered, and any ship attempting to proceed to sea in

such circumstances shall be deemed to be attempting to proceed to sea without a certificate, and shall be dealt with accordingly.

15. The master or owner of every ship shall, as soon as possible, report to the Assigning Authority who gave such ship a load line certificate, any damage to the hull or superstructures which, under the provisions of the preceding section, would cause such load line certificate to cease to have force.

16. A load line certificate which has been cancelled, or has ceased to have force, shall be delivered up by the owner or master of the ship to the Assigning Authority by whom it was issued.

17. (1) A load line certificate which has been issued for any period less than five years may be extended by the Assigning Authority which issued it, provided it is satisfied that it can, with propriety, do so, and provided:—

(a) that the ship is surveyed as required for annual survey under the provisions of Section 11 of these regulations.

(b) that no extension shall cause the certificate to remain in force for a period in excess of five years from the date of issue.

(2) The extension of a certificate under the provisions of this section may be indicated by an endorsement on the certificate, or by the issue of an extension certificate, and the endorsement, or the extension certificate, may be issued over the signature of a surveyor employed by the Assigning Authority.

(3) The fee chargeable for annual survey under the provisions of Section 12 shall be paid for the first extension of a certificate granted under this section, for any period in excess of two months.

18. The following sections, 19 to 23 inclusive, shall have effect where the Assigning Authority is the Minister.

19. Application for the assignment of load lines, or for the renewal of a load line certificate, shall be made in duplicate, on forms, "Application for survey for load lines," which may be obtained from any Steamship Inspector, or from the Department of Marine.

20. Applications for the assignment of load lines in respect of any ship shall be filled out to show all the particulars therein demanded, and shall be addressed to the office of the Steamship Inspector in the district in which the ship will be surveyed, or, where a ship is not to be surveyed in Canada, to the Chairman.

21. Where a ship, in respect of which application is made for the assignment of load lines, is one classed in a recognized classification society, the certificate of class, or a certified copy thereof, shall be attached to the application.

22. (1) Where a ship is not one classed to the highest standard of a recognized classification society, the application for the assignment of load lines shall, subject to the provisions of this section, be accompanied by plans, properly dimensioned, and sufficient in detail, to the satisfaction of the Chairman, to show the structural strength of the ship, the arrangement of superstructures, and the arrangement, spacing and construction of the bulkheads, specifying which of these are watertight.

(2) If it be established to the satisfaction of the Chairman that in any particular case it would be unreasonable to demand the production of plans, as described in subsection (1) of this section, this provision need not be insisted on, but in such case the fees for the assignment of load lines shall be increased in accordance with the scale shown in the first schedule to these regulations.

23. Where a ship is not classed in a recognized classification society, and is surveyed for the assignment of load lines, she shall be subject to such opening out, drilling of plates and such like, as would be required in accordance with her age, under the rules for classification or continuance in class of one of the classification societies authorized as an Assigning Authority under the provisions of these regulations.

24. When a load line certificate has been issued in pursuance of the provisions of the Act, the owner shall forthwith, on receipt thereof, cause it to be framed and posted up in some conspicuous place on board the ship, and to be kept so framed and posted up and legible so long as the certificate remains in force and the ship is in use.

25. (1) Except as is provided in subsection (2) of this section, the master of every ship registered in Canada shall, before such ship leaves any dock, wharf, harbour or other place, for the purpose of proceeding to sea, enter in the official log book:—

- (i) if not previously entered, all the particulars stated in the load line certificate of the position of the deck line and the load lines, together with the draught of water which would be shown on the scale of feet on the stem and sternpost of the ship if she were so loaded that the upper edge of the summer load line marked on the ship in accordance with the provisions of the Convention were on the surface of the water and the ship were upright on an even keel
- (ii) the actual draught of water of the ship as shown on the scale of feet on her stem and sternpost when the ship is loaded and ready to leave the said dock, wharf, harbour, or other place, together with the actual freeboard amidships on each side of the ship, and the mean freeboard (the mean of the freeboards amidships on each side of the ship)
- (iii) the density of the water in which the ship is lying at the time when the particulars required by the foregoing paragraph (ii) are ascertained.
- (iv) the allowances, if any, to be made in order to arrive at the mean freeboard which the ship will have when she first reaches salt water after leaving, as aforesaid, in respect of:—
 - (a) the density of the water in which the ship was lying at the time when the freeboard was ascertained, in pursuance of subsection (1) (ii) of this section
 - (b) the weight of any ashes and rubbish which it is intended to throw overboard
 - (c) the weight of fuel, water and stores required for consumption before the ship reaches salt water
- (v) the mean draught of water and the mean freeboard which the ship will have in salt water, as calculated after making the aforesaid allowances

(2) The entries required by subsection (1) (iii), (iv) and (v) of this section need not be made if, at the time when the particulars required by subsection (1) (ii) are ascertained, the load line indicating the maximum depth to which the ship is for the time being entitled to be loaded in salt water is not submerged.

(3) The master of every ship registered in Canada shall, before leaving as aforesaid, enter in the official log book the date and time of posting the notice required to be posted by Section 26 of these regulations.

26. (1) Before any ship registered in Canada leaves any dock, wharf, harbour or other place for the purpose of proceeding to sea, the master thereof

shall cause a notice setting out particulars relating to draught of water and freeboard to be posted up in some conspicuous place on board the ship, and to be kept so posted up and legible until the ship arrives at some other dock, wharf, harbour or place.

(2) The notice to be posted up in accordance with this section shall, in the case of a ship other than one marked with timber load lines in accordance with the provisions of the Convention, be in form No. 1, set out in the third schedule to these regulations, and, in the case of a ship marked with timber load lines, in form No. 2, set out in the same schedule, and shall show all the particulars therein provided for.

(3) Each entry in such notice, under the heading "Particulars of Loading", shall be signed by the master and first mate.

27. Before an agreement with the crew of any ship registered in Canada in respect of which a load line certificate is in force is signed by any member of the crew, the master of the ship shall insert in the agreement the particulars as to the position of the deck line and the load lines specified in the certificate, and a Shipping Master shall not proceed with the engagement of the crew until:—

- (a) there is produced to him a load line certificate for the time being in force in respect of the ship, and
- (b) he is satisfied that the particulars required by this section have been inserted in the agreement with the crew.

28. The following sections, 29 to 32, inclusive, refer to steamers of any registry, marked with timber load lines, which are at a port or place in Canada, and are loaded beyond the maximum depth to which they would, for the time being, be entitled under the provisions of the Convention, to be loaded, if they were not marked with timber load lines.

29. A steamer shall not be taken to sea unless she has been surveyed by a Port Warden or other person directed thereto by the Minister, and such Port Warden or other person has given a certificate showing that the provisions of the Convention in respect of the carriage of deck cargoes of timber have been complied with.

30. It shall be the duty of the owner or master of any steamer to have her surveyed in accordance with the provisions of the preceding section, 29, and to give due notice to the Port Warden or other such person, of the intention to load a deck cargo of timber and otherwise give all necessary facilities for such survey.

31. (1) Except as provided in this section a Port Warden, or other person directed by the Minister to make survey under the provisions of Section 29, shall be entitled to a fee of \$10.00 in respect of each certificate he issues, which shall be paid by the owner or master before such certificate is issued.

(2) The owner or master of any steamer shall not be liable for a fee in respect of any survey made under the provisions of Section 29, where such survey is made by a Port Warden, if such Port Warden is entitled to any other fee in respect of the loading of timber in such ship.

32. A Collector of Customs shall not give a clearance to any steamer unless there be produced to him a certificate in respect of deck cargoes of timber, as required under the provisions of Section 29.

33. The material of the tarpaulins for hatchways required under Rule XV of the Convention shall have the minimum weights per square yard, before treatment, as follows:—

- 19 ounces, if to be tarred
- 18 ounces, if to be chemically dressed
- 16 ounces, if to be dressed with black oil

FIRST SCHEDULE

Gross tonnage	Fees						
	Classed Ships			Unclassed Ships			
	1 Issue of certifi- cate	2 Renewal of certifi- cate	3 Annual survey	4 Issue of certifi- cate	5 Renewal of certifi- cate	6 Annual survey	
	\$	cts.	\$	cts.	\$	cts.	
Under							
500 tons.	30	00	10	00	10	00	
1,000 "	40	00	10	00	75	00	
2,000 "	55	00	15	00	110	00	
3,000 "	65	00	17	50	140	00	
4,000 "	70	00	20	00	155	00	
5,000 "	75	00	20	00	170	00	
6,000 "	80	00	20	00	185	00	
7,000 "	85	00	25	00	200	00	
8,000 "	90	00	25	00	215	00	
9,000 "	95	00	25	00	230	00	
10,000 "	100	00	25	00	245	00	
10,000 tons and over...	100	00	25	00	245	00	
				plus \$15.00 for each additional 1,000 tons	plus \$15.00 for each additional 1,000 tons	20	00

For barges, scows or other such vessels towed, under 1,000 tons, gross tonnage, fees as follows shall be charged:—

For the issue of a certificate...	...	...	...	...	...	\$30 00
For the renewal of a certificate...	...	...	...	...	...	10 00
For annual survey...	...	...	...	...	...	10 00

APPLICATION OF FEES

1. In the case of the survey of a classed ship for the renewal of the load line certificate, the fee in column 2 will be charged if the survey is carried out by the society in which the ship is classed, and if it is carried out concurrently with the special survey for classification purposes for which a fee is charged; otherwise, the fee will be 50 per cent of that in column 1.

2. (a) In the case of the survey of an unclassed steamer for the issue or renewal of a load line certificate, where the Assigning Authority is the Minister, half the fee set out in column 4 will be charged if the survey be carried out concurrently with the annual inspection required under the provisions of Part VII of the Canada Shipping Act.

(b) If the load line certificate is one issued where the Minister is the Assigning Authority, and the annual load line survey is made at the same time as the annual inspection required under the provisions of the Canada Shipping Act, no fee will be charged under column 3 or column 6.

3. Where minor alterations have been made to a ship having a load line certificate in force, which involve an alteration of the freeboard, but do not require a full survey, the fee in column 2 will be charged.

4. In the case where the Minister is the Assigning Authority and plans as required under the provisions of Section 22 of these regulations are not submitted, fees for the assignment of load lines will be increased by the addition of the following:—

For ships under 2,000 tons...	...	...	...	...	\$20 00
" " of 2,000 and under 6,000 tons...	...	...	...	...	30 00
" " of 6,000 tons and over...	...	...	...	...	50 00

SECOND SCHEDULE

FORM L.L. 2



Issued by the
Department of
Marine, Canada.

INTERNATIONAL LOAD LINE CERTIFICATE

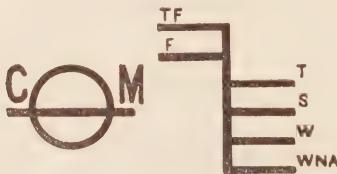
Issued under the authority of the Government of the
Dominion of Canada under the provisions of the Interna-
tional Load Line Convention, 1930.

Ship's name Official number.....

Port of registry.....

Gross tonnage

Freeboard from deck line	Load line
Tropical .....feet...inches (T).	.....inches above S.
Summer .....feet...inches (S).	Upper edge of line through centre of disc.
Winter .....feet...inches (W).	.....inches below S.
Winter north Atlantic...feet...inches (WNA).	.....inches below S.
Allowance for fresh water for all freeboards.....inches.	
The upper edge of the deck line from which these freeboards are measured is.....inches above the top of the .....deck at side.	



This is to certify that this ship has been surveyed and the freeboards and
load lines shown above have been assigned in accordance with the Convention.
This certificate remains in force until.....
Issued at Ottawa on the.....day of.....19....

Registered

Chairman, Board of Steamboat Inspection.

(Seal of Minister)

NOTE.—Where seagoing steamers navigate a river or inland water, deeper loading is per-
mitted, corresponding to the weight of fuel, etc. required for consumption between
the point of departure and the open sea.

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force, and the survey has been completed to my satisfaction.

Signature of surveyor.....

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force, and the survey has been completed to my satisfaction.

Signature of surveyor.....

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force, and the survey has been completed to my satisfaction.

Signature of surveyor.....

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force, and the survey has been completed to my satisfaction.

Signature of surveyor.....

Place Date

NOTES

1. This certificate must be kept framed and posted up in some conspicuous part of the ship so long as it remains in force, and the ship is in use.

2. The Winter North Atlantic load line applies for voyages across the North Atlantic, north of latitude 36° N., during the winter months as defined in the Convention. The periods during which the other seasonal load lines apply in different parts of the world, are as stated in the Convention.

3. This certificate will be cancelled by the Minister of Marine if:—

- (a) material alterations have taken place in the hull or superstructures of the ship which affect the position of the load lines; or
- (b) the fittings and appliances for the protection of openings, the guard rails, the freeing ports or the means of access to the crew's quarters have not been maintained on the ship in as effective a condition as they were in when the certificate was issued; or
- (c) the annual survey is not made as required by the Load Line Regulations.

4. Where this certificate has expired or been cancelled, it must be delivered up to the Assigning Authority, and the ship may be detained until such requirement has been complied with, and if any owner or master fails without reasonable cause to comply with such requirement, he shall, for each offence, be liable to a fine not exceeding five hundred dollars.

APPLIANCES FOR CLOSING ACCESS OPENINGS IN BULKHEADS AT ENDS OF DETACHED SUPERSTRUCTURES

Forecastle

Bridge, fore end

Bridge, after end

Raised quarter deck

Poop

TEMPORARY APPLIANCES FOR CLOSING OPENINGS IN SUPERSTRUCTURE DECKS

.....

FORM L.L. 2A



Issued by the
Department of
Marine, Canada.

INTERNATIONAL LOAD LINE CERTIFICATE

Issued under the authority of the Government of the
Dominion of Canada under the provisions of the Interna-
tional Load Line Convention, 1930.

Ship's name Official number.....

Port of registry

Registered tonnage

Freeboard from deck line

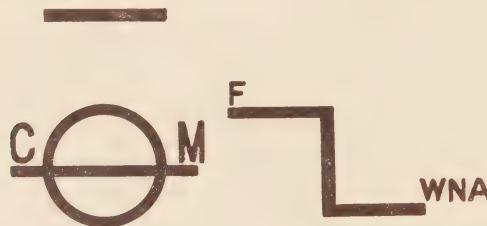
Load line

Tropical }
Summer }.....feet,inches. Upper edge of line through centre of disc.
Winter }

Winter North Atlantic.....feet,.....inches (WNA).inches below upper
edge of line through
centre of disc.

Allowance for fresh water for all freeboards:.....inches.

The upper edge of the deck line from which these freeboards are measured
is..... inches above the top of the..... deck at side.



This is to certify that this ship has been surveyed and the freeboards and load
lines shown above have been assigned in accordance with the Convention.

This certificate remains in force until.....

Issued at Ottawa on theday of 19..

Registered

Chairman, Board of Steamboat Inspection.

(Seal of Minister)

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

NOTES

1. This certificate must be kept framed and posted up in some conspicuous part of the ship so long as it remains in force and the ship is in use.

2. The Winter North Atlantic load line applies for voyages across the North Atlantic, north of latitude 36° N., during the winter months as defined in the Convention. The upper edge of the line through the centre of the disc applies for all other voyages.

3. This certificate will be cancelled by the Minister of Marine if:—

- (a) material alterations have taken place in the hull or superstructures of the ship which affect the position of the load lines; or
- (b) the fittings and appliances for the protection of openings, the guard rails, the freeing ports or the means of access to the crew's quarters have not been maintained on the ship in as effective a condition as they were in when the certificate was issued; or
- (c) the annual survey is not made as required by the Load Line Regulations.

4. Where this certificate has expired or been cancelled, it must be delivered up to the Assigning Authority and the ship may be detained until such requirement has been complied with, and if any owner or master fails, without reasonable cause, to comply with such requirement, he shall, for each offence, be liable to a fine not exceeding five hundred dollars.

APPLIANCES FOR CLOSING ACCESS OPENINGS IN BULKHEADS AT ENDS OF DETACHED SUPERSTRUCTURES

Forecastle

Bridge, fore end

Bridge, after end

Raised quarter deck

Poop

TEMPORARY APPLIANCES FOR CLOSING OPENINGS IN SUPERSTRUCTURE DECKS

.....
.....

FORM L.L. 2B



Issued by the
Department of
Marine, Canada.

INTERNATIONAL LOAD LINE CERTIFICATE

Issued under the authority of the Government of the
Dominion of Canada under the provisions of the Interna-
tional Load Line Convention, 1930.

Ship's name Official number

Port of registry

Gross tonnage

Freeboard from deck line

Load line

Tropical feet inches (T) inches above S.

Summer feet inches (S) Upper edge of line
through centre of disc.

Winter feet inches (W) inches below S.

Winter North Atlantic feet inches (WNA) inches below S.

Allowance for fresh water for all freeboards inches.

The following load lines are applicable only when the ship is carrying a timber deck cargo and complies with all the provisions of the Convention respecting the carriage of deck cargoes of timber.

Freeboard from deck line

Load line

Tropical (Timber) feet inches (LT) inches above LS.

Summer (Timber) feet inches (LS) inches above S.

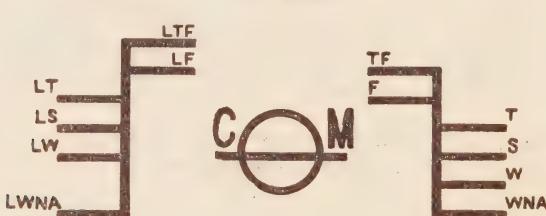
Winter (Timber) feet inches (LW) inches below LS.

Winter North

Atlantic (Timber) feet inches (LWNA) inches below LS.

The upper edge of the deck line from which these freeboards are measured is

..... inches above the top of the deck at side.



This is to certify that this ship has been surveyed and the freeboards and load lines shown above have been assigned in accordance with the Convention.

This certificate remains in force until

Issued at Ottawa on the day of 19....

Registered.....

Chairman, Board of Steamboat Inspection.

(Seal of Minister)

NOTE.—Where sea-going steamers navigate a river or inland water, deeper loading is permitted, corresponding to the weight of fuel, etc., required for consumption between the point of departure and the open sea.

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

I have surveyed this ship for the purpose of seeing whether this certificate should remain in force and the survey has been completed to my satisfaction.

Signature of surveyor

Place Date

NOTES

1. This certificate must be kept framed and posted up in some conspicuous part of the ship so long as it remains in force and the ship is in use.

2. The Winter North Atlantic load lines apply for voyages across the North Atlantic, north of latitude 36° N., during the winter months as defined in the Convention. The periods during which the other seasonal load lines apply in different parts of the world are as stated in the Convention.

3. This certificate will be cancelled by the Minister of Marine if:—

(a) material alterations have taken place in the hull or superstructures of the ship which affect the position of the load lines; or

(b) the fittings and appliances for the protection of openings, the guard rails, the freeing ports or the means of access to the crew's quarters have not been maintained on the ship in as effective a condition as they were in when the certificate was issued; or

(c) the annual survey is not made as required by the Load Line Regulations.

4. Where this certificate has expired or been cancelled, it must be delivered up to the Assigning Authority and the ship may be detained until such requirement has been complied with, and if any owner or master fails, without reasonable cause, to comply with such requirement, he shall, for each offence, be liable to a fine not exceeding five hundred dollars.

APPLIANCES FOR CLOSING ACCESS OPENINGS IN BULKHEADS AT ENDS OF DETACHED SUPERSTRUCTURES

Forecastle

Bridge, fore end

Bridge, after end

Raised quarter deck

Poop

TEMPORARY APPLIANCES FOR CLOSING OPENINGS IN SUPERSTRUCTURE DECKS

THIRD SCHEDULE

Form L.L. 14 A.

Issued by the
Department of
Marine, Canada.

FORM No. 1

NOTICE

DRAUGHT OF WATER AND FREEBOARD

Ship of Gross tonnage.....

1. Summer freeboard†feet,inches, corresponding to a mean draught‡ offeet,inches.
2. Winter freeboard†feet,inches, corresponding to a mean draught offeet,inches.
3. Tropical freeboard†feet,inches.
4. Winter North Atlantic freeboard† (if assigned)feet,inches.
5. Allowance for fresh water for all freeboards‡inches.

†Particulars to be taken from load line certificate.

‡The mean draught to be entered is the draught of water which would be shown on the scale of feet on the stem and stern post of the ship if she were so loaded that the upper edge of the summer load line were on the surface of the water and the ship were upright on an even keel.

PARTICULARS OF LOADING

Date	Place	Actual draught			Mean freeboard		Signature of master and first mate	
		Forward	Aft	Mean	Actual (See note below)	Corrected (See note below)	Master	Mate
1	2	3	4	5	6	7	8	9

NOTES

The mean freeboard (actual) is the mean of the freeboards measured from the top of the deck line specified in the load line certificate to the water on each side of the ship when loaded and ready to leave.

If the actual mean freeboard is less than the appropriate salt water freeboard, the master must enter in column 7 the corrected freeboard arrived at after making any allowances claimed in respect of density of water, ashes and rubbish to be thrown overboard and fuel, etc., to be consumed on any stretch of river or inland water. Such allowances must be entered in the ship's official log book.

If the actual mean freeboard is not less than the appropriate salt water freeboard, column 7 need not be filled in and no entry as to allowances need be made in the official log book.

Issued by the
Department of
Marine, Canada.

FORM No. 2

NOTICE

DRAUGHT OF WATER AND FREEBOARD

Ship of Gross tonnage

1. Summer freeboard[†]feet,inches, corresponding to a mean draught[‡] offeet,inches.
2. Winter freeboard[†]feet,inches, corresponding to a mean draught offeet,inches.
3. Tropical freeboard[†]feet,inches.
4. Winter North Atlantic freeboard[†] (if assigned)feet,inches.
5. Allowance for fresh water for all freeboards[†]inches.
6. Summer timber freeboard[†]feet,inches, corresponding to a mean draught offeet,inches.
7. Winter timber freeboard[†]feet,inches, corresponding to a mean draught offeet,inches.
8. Tropical timber freeboard[†]feet,inches.
9. Winter North Atlantic timber freeboard[†]feet,inches.

[†]Particulars to be taken from load line certificate.

[‡]The mean draught to be entered is the draught of water which would be shown on the scale of feet on the stem and stern post of the ship if she were so loaded that the upper edge of the summer load line were on the surface of the water and the ship were upright on an even keel.

PARTICULARS OF LOADING

Date	Place	Actual draught			Mean freeboard		Signature of master and first mate	
		Forward	Aft	Mean	Actual (See note below)	Corrected (See note below)	Master	Mate
1	2	3	4	5	6	7	8	9

NOTES

The mean freeboard (actual) is the mean of the freeboards measured from the top of the deck line specified in the load line certificate to the water on each side of the ship when loaded and ready to leave.

If the actual mean freeboard is less than the appropriate salt water freeboard, the master must enter in column 7 the corrected freeboard arrived at after making any allowances claimed in respect of density of water, ashes and rubbish to be thrown overboard and fuel, etc., to be consumed on any stretch of river or inland water. Such allowances must be entered in the ship's official log book.

If the actual mean freeboard is not less than the appropriate salt water freeboard, column 7 need not be filled in and no entry as to allowances need be made in the official log book.

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